



East Tuolumne master
City of Turlock, Ca *plan*



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The East Tuolumne Master Plan was originally prepared for the City of Turlock by William Hezmahalch Architects Inc.

This Master Plan Amendment has been prepared by City of Turlock Staff, with updated Figures provided by GDR Engineering Inc.

Chapter 1 - Introduction

1.1 Purpose

The City of Turlock is committed to community planning to create an attractive and pleasant living environment and enhance the quality of life for current and future residents. This is evident in the City's General Plan, which not only addresses basic land use, circulation and housing needs in accordance with state law, but also dedicates an entire Element to City Design. The East Tuolumne Master Plan's purpose is to implement the General Plan's goals and policies while further refining a vision for the Master Plan area, thus ensuring cohesive and attractive residential development.

The East Tuolumne Master Plan area is located at the easterly edge of the City limits. The City of Turlock General Plan designates this area for residential development and also recognizes this area as the City's urban-agricultural edge.

The General Plan, updated in 2012, identified evaluating and updating the East Tuolumne Master Plan area as a high priority. Since the Master Plan's adoption in 2005 market conditions have not supported developing the land with very low-density residential uses. Increasing the allowable density in this Master Plan area by re-designating two of the parcels to Low Density Residential, will reduce the per unit development impact fee cost and should help make development in this area financially feasible.

Another major theme of the Turlock General Plan is the preservation of farmland through more compact development.

The primary purpose of the East Tuolumne Master Plan update is to implement the General Plan's requirement for an agriculture buffer while also implementing the General Plan policy promoting more compact growth by increasing the number of lots to be developed than was allowed for in the original Master Plan.

This update will increase the units from approximately 200 to 278. This will increase the density while still maintaining larger lots than typically found in residential areas.

The City seeks to create a smooth transition from the more dense residential development in the west and south to the rural development within the County in the east and north, while allowing for slightly more dense development than the very low-density residential land use designation. The overall goal of the Master Plan is to establish a clear separation between Turlock and Denair and ensure that both remain distinct and independent communities.

The Master Plan intends to create neighborhoods with a rural character and strong sense of community. Rural areas are characterized by large estate homes, a variety of lot widths, traditional architectural styles, large setbacks, and open fencing. The Master Plan establishes design guidelines and development standards that not only ensure a consistent rural character throughout the Master Plan area but also ensure effective integration with existing neighborhoods.

The Master Plan provides a framework for developers to follow when preparing development proposals. Further, it establishes criteria for City staff and policy makers to use when evaluating the consistency and appropriateness of development proposals. In most cases, the standards developed in the Master Plan are consistent with existing citywide development standards. Where there is a discrepancy between citywide standards and standards within the Master Plan, it is intended that the standards within the Master Plan shall apply. The Master Plan does not seek to restrict development but rather seeks to enhance the quality and character of future residential development for this portion of Turlock.

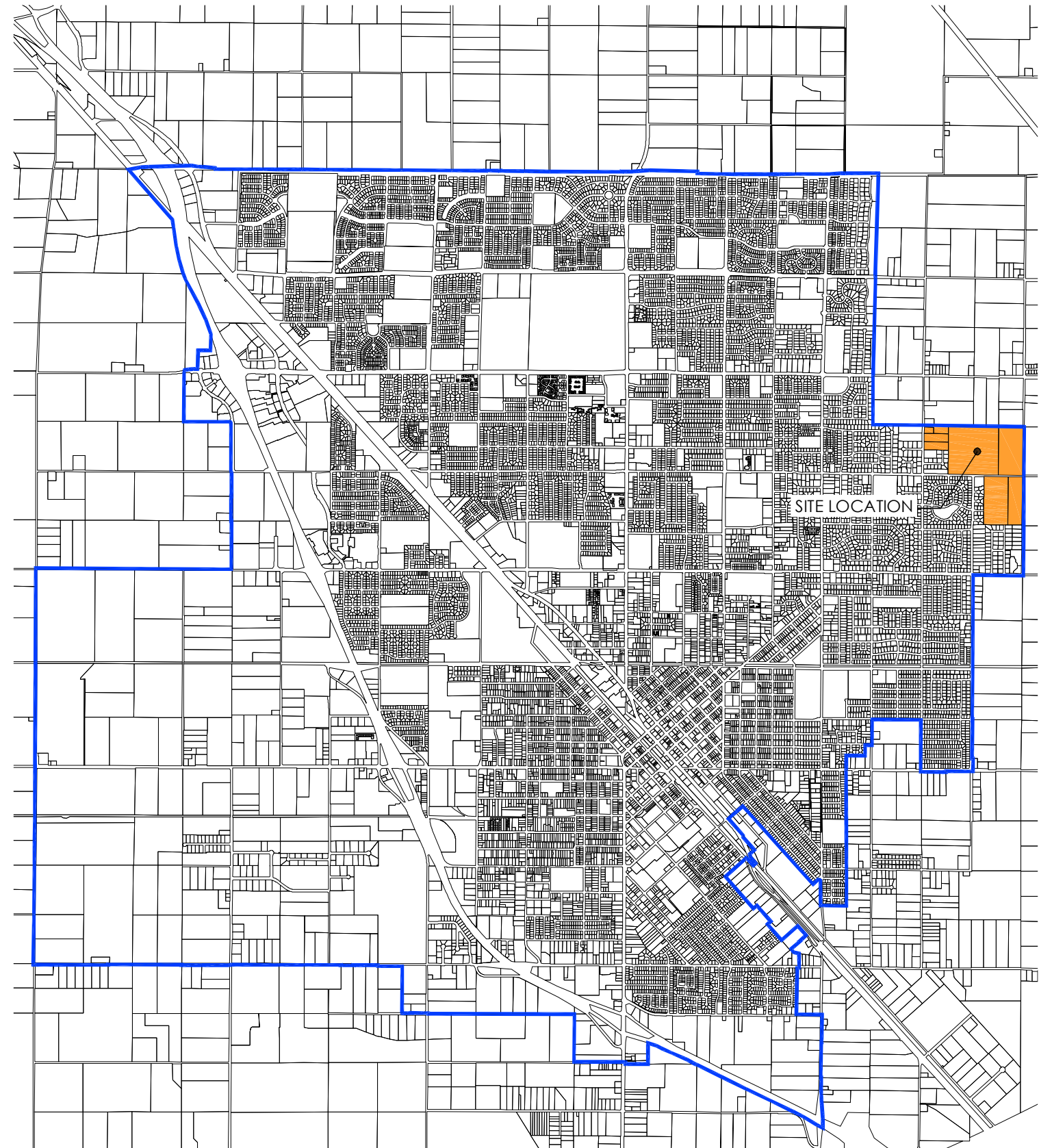


Figure 1-1
Master Plan Location Map

Chapter 1 - Introduction

1.2 Planning Area

The City of Turlock is located in the southern portion of Stanislaus County along Highway 99. The 101.3 acre East Tuolumne Master Plan area was annexed into the City in 2006. The northern and eastern edge of the master plan area is the City limit, between North Quincy Road and North Waring Road. East Tuolumne Road runs through the planning area in an east-west direction dividing the planning area into two distinct sections. See Figure 1-1 on the previous page, which shows the location of the Master Plan area.

1.3 Master Plan Ownership

The Master Plan area consists of eight parcels, each individually owned. Two parcels located south of East Tuolumne Road total approximately 32 acres. The remaining six parcels to the north include one 19.7 acre parcel, one 40.6 acre parcel and four smaller parcels ranging from approximately 1-3 acres in size. See Figure 1-2 on the following page, which shows the various Master Plan properties and their corresponding ownerships.

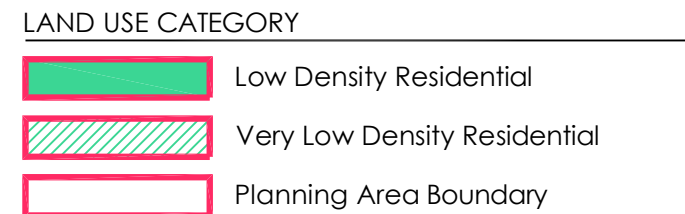
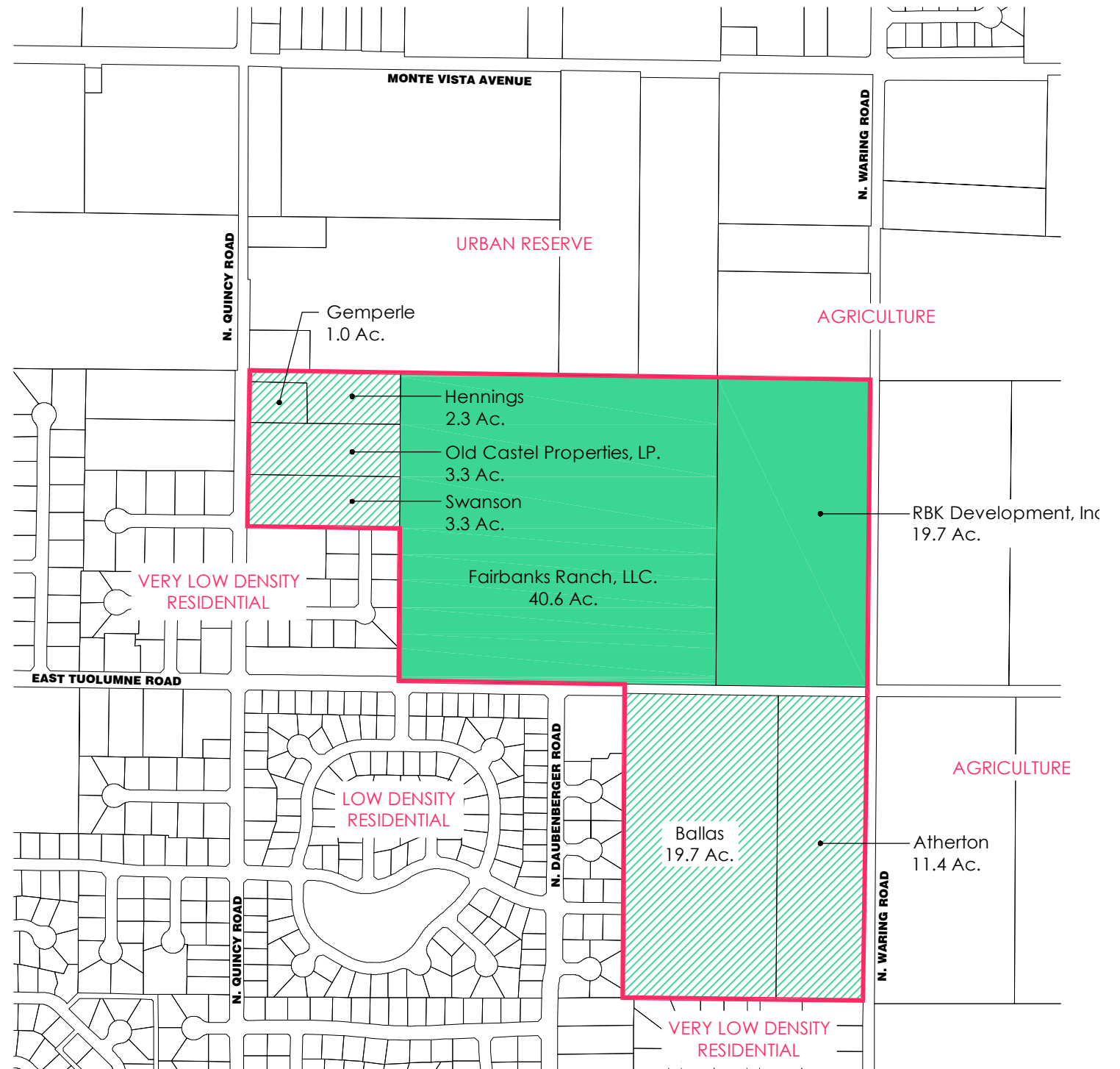
1.4 Existing Conditions

The Master Plan area consists of relatively flat terrain and is utilized primarily for agricultural uses. A limited number of estate homes have been constructed in the planning area along North Quincy Road and East Tuolumne Road. The southerly portion of the site includes three houses and seasonal crops while the northerly portion of the site predominantly consist of seasonal crops.

The planning area is surrounded by low to very low-density residential development and agricultural lands. Existing low-density residential neighborhoods to the south and southeast consist of 5000 SF minimum lot subdivisions. Homes in these neighborhoods generally include production

homes and custom homes of various height, mass and architectural style. As residential development continues toward the northerly and easterly city limits, residential subdivisions give way to larger custom estate lots on minimum 14,500 SF lots. Agricultural uses to the north include orchards and fallow fields. These lands are located outside the City limit but are within the City's Sphere of Influence. The City of Turlock's General Plan has designated this area as "Urban Reserve".

Agricultural uses to the east include vineyards and orchards, which are located outside both the City's corporate limit and Sphere of Influence. Figure 1-3 demonstrates the existing land uses and physical features within the Master Plan area and Figure 1-4 includes images of existing land uses within and surrounding the site. Both exhibits are shown on the following pages.



101.3 Total Acres

Figure 1-2
Master Plan Properties



- LEGEND**
- Planning Area Boundary
 - Existing Estate Home
 - Existing Intersections
 - Existing Roads

Figure 1-3
Existing Land Uses



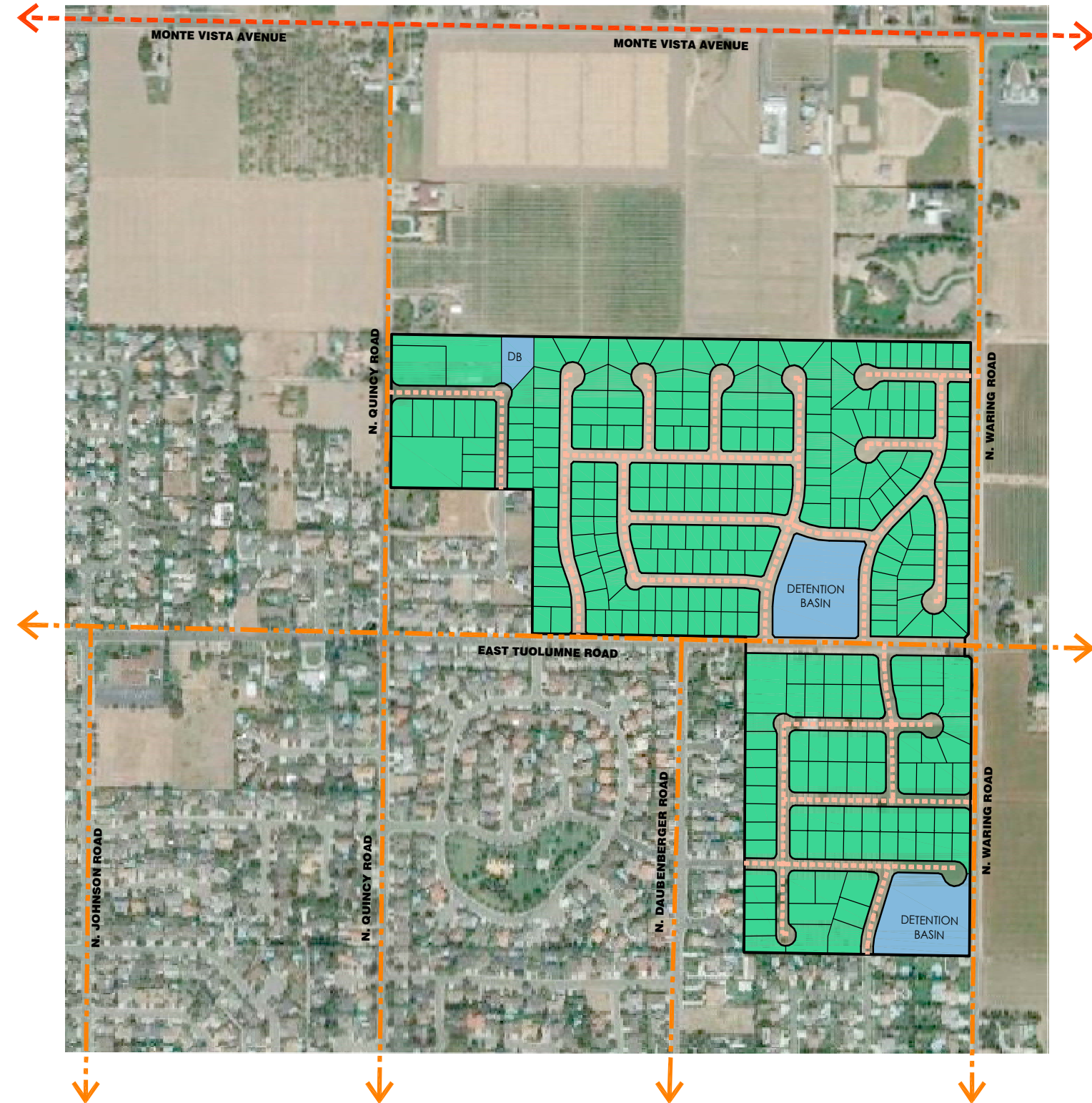
Figure 1-4
Existing Imagery

Chapter 2 - Circulation

2.1 Introduction

The planning area is currently served by three major roadways: East Tuolumne Road divides the Master Plan area in an east-west direction, North Waring Road serves as the project's easterly boundary, and North Quincy Road serves as the project's westerly boundary. The small residential subdivision adjacent to the northwest corner of the planning area provides a stub street connecting to the four smallest parcels.

The Master Plan proposes minor changes to the existing circulation pattern. These changes will primarily consist of frontage and intersection improvements as described in later sections of this chapter. New local roadways and intersections will be required to serve future residential development. See Figure 2-1 on the following page, which demonstrates the existing and proposed circulation pattern within and surrounding the Master Plan area.



- GENERAL PLAN
VEHICULAR CIRCULATION
- Existing Arterial
 - - - - Existing 2-Lane Collector
Includes Class II Bike Lanes (70' R.O.W.)
 - Potential Future Local Road Alignment

Figure 2-1
Conceptual
Circulation Plan

Chapter 2 - Circulation

2.2 Roadway Character and Circulation Design Guidelines

The Master Plan circulation network aims to provide an interconnected system of roadways, sidewalks and bike paths, unifying the various parcels into one neighborhood while allowing efficient movement of vehicles, pedestrians and bicyclists through the planning area. The following design guidelines are intended to provide a safe, efficient, and interconnected circulation system for the Master Plan area:

- a. Streets should be curvilinear to enhance the streetscene, encourage reduced traffic speeds, and create a more rural atmosphere.
- b. The circulation network should be designed to provide multiple vehicular and pedestrian connections between the various properties within the Master Plan area to establish the entire area as one unified neighborhood and encourage pedestrian or bicycle use as an alternative to vehicular use.
- c. Cul-de-sacs should be minimized whenever possible.
- d. All public streets shall be improved along the Master Plan area's frontage and dedicated to the City of Turlock.
- e. The City of Turlock shall be responsible for the repair and maintenance of all public streets within the planning area.
- f. North Waring Road and East Tuolumne Road shall be improved to include Class II bicycle lanes.
- g. Sidewalks shall be provided on both sides of all local public streets within the planning area.
- i. Landscaping within public right-of-ways shall be consistent with the landscape design guidelines provided in Chapter 3 of this Master Plan.
- j. A Landscape and Lighting District (LLD) shall be established to maintain not only standard public improvements, but also entry monumentation, special paving, decorative lighting and other non-standard design elements.
- k. Pavement sections for public streets shall be in

accordance with minimum City standards and the requirements of the City Engineer.



2.3 Collector Streets

Collector streets are larger streets intended to serve higher volumes of traffic with multiple turning movements. Collector streets provide connections between heavily traveled arterials and local streets. Three collector streets currently serve the Master Plan area: North Waring Road, East Tuolumne Road, and North Quincy Road.

2.3.1 North Waring Road

Policy 6.1-k of the City of Turlock General Plan calls for the implementation of an "agricultural-urban buffer design to minimize the impact of urban development near active agricultural operations. This policy restricts housing from fronting onto agricultural properties. In accordance with this Policy, the Master Plan proposes a block wall along North Waring Road with no houses fronting onto North Waring Road. This will help to create a buffer between the urban edge and the agricultural uses across North Waring Road to minimize urban-agriculture conflicts.

The street section for North Waring Road consists of a 56'-foot right of way including one 20-foot travel lane, one 5-foot Class II bike lane, 6-foot landscape strip and 5-foot sidewalk on the west side of Waring. Parking will not be permitted along the western side of Waring Road. The fifteen-foot

public utility easement shall be landscaped. The eastern side of Waring Road will have one 12-foot travel lane, one 5-foot Class II bike lane and a 3-foot swale. The swale-type drainage gutter with rock or gravel bottom is encouraged as an alternative to typical concrete curb and gutter in order to maintain a more rural appearance. Figure 2-2 on the following page illustrates the street section for North Waring Road.

2.3.2 North Quincy Road

The street section for North Quincy Road will consist of a 60-foot right-of-way including two 12-foot travel lanes, two 8-foot parking lanes, two Class III bike routes, 5-foot sidewalks, five-foot landscape strips on both sides, and City standard curb and gutter. The remaining street section will consist of landscaping as shown in Figure 2-3 on the following page.

2.3.3 East Tuolumne Road

East Tuolumne Road will be improved along the planning area's frontage to meet City standards and will include a Class II bikeway. Improvements to East Tuolumne Road will include dedication of additional right-of-way and the installation of curb, gutter and sidewalk along the planning area's frontage to be consistent with existing improvements to the west.

There are three different street sections for East Tuolumne Road to accommodate existing development, lots that front onto East Tuolumne Road and lots that back onto East Tuolumne Road.

Figure 2-4 shows the first street section for East Tuolumne Road that will extend from Tiverton Way up to APN 073-016-006 on the south side of the street where the exiting frontage improvements will remain. Figure 2-5 also shows the new improvements that will be installed along the north side of East Tuolumne Road for lots fronting onto East Tuolumne Road and the area adjacent to the drainage basin.

This section will consist of a 71-foot right-of-way including two 12-foot travel lanes, two 5-foot Class II bike lanes and two 8-foot parking lanes.

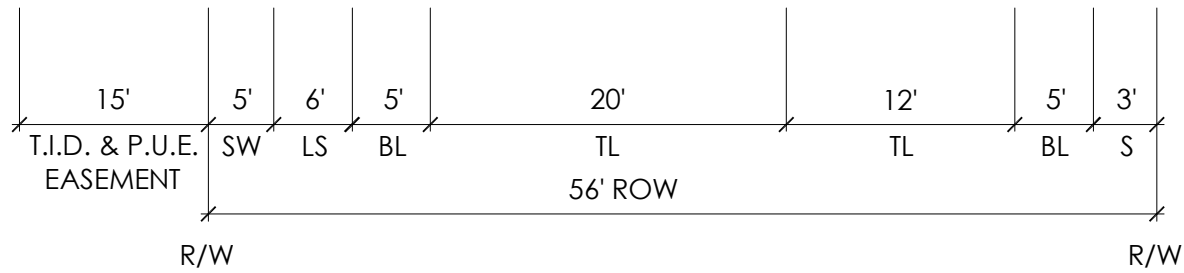
Chapter 2 - Circulation

The existing improvements on the south side of East Tuolumne Road consist of a 5-foot sidewalk and a 5-foot landscape strip and City standard curb and gutter. The improvements on the north side of East Tuolumne Road will include a 6-foot landscape strip adjacent to the parking lanes and a 5-foot sidewalk.

Figure 2-6 shows the improvements that will be installed along the south side of East Tuolumne Road along parcel 073-016-006 & 073-016-007 and the improvements on the north side of East Tuolumne Road for any lots fronting onto East Tuolumne Road and the area adjacent to the drainage basin.

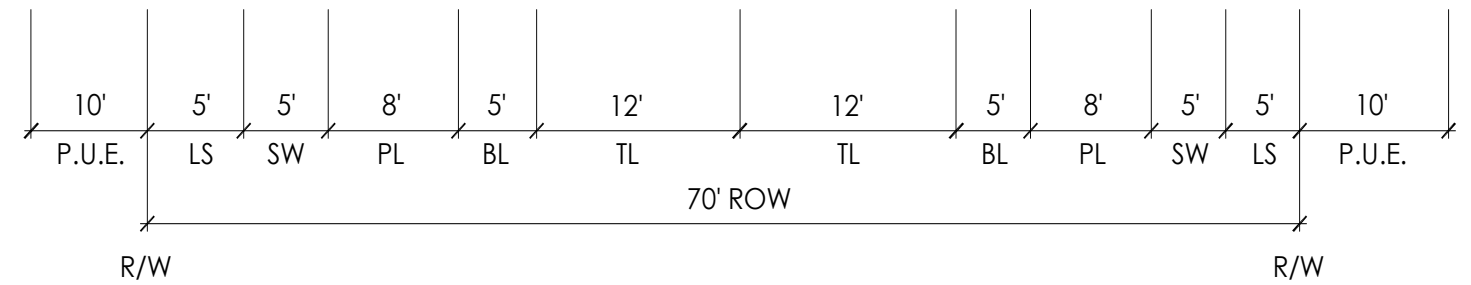
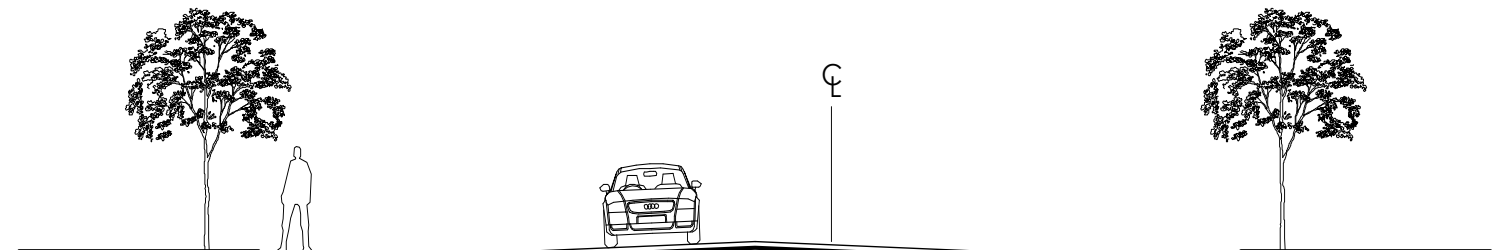
This section will consist of a 72-foot right-of-way including two 12-foot travel lanes, two five-foot Class II bike lanes, two 8-foot parking lanes, two six-foot landscape strips and two five-foot sidewalks.

Figure 2-7 will consist of the same improvements as Figure 2-6 with the exception of a 15-foot public utility easement that will be required along the properties on the north side of East Tuolumne Road that back onto the road with a masonry wall and adjacent to East Tuolumne Road. The masonry wall will be setback 15 feet from the public right-of-way and landscaping will be installed in this area.

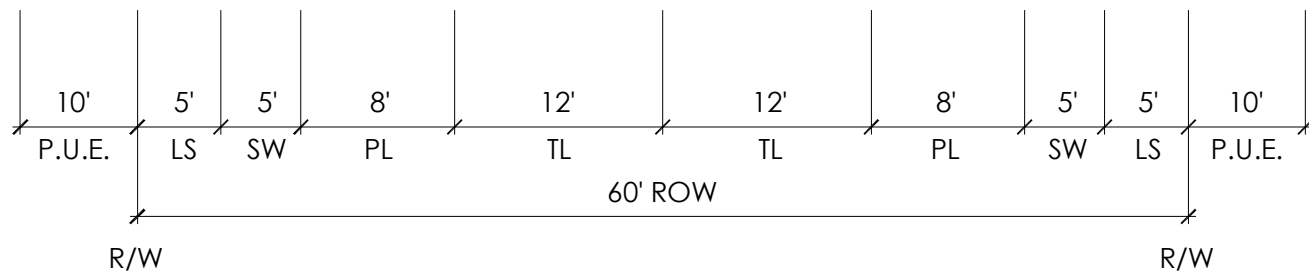
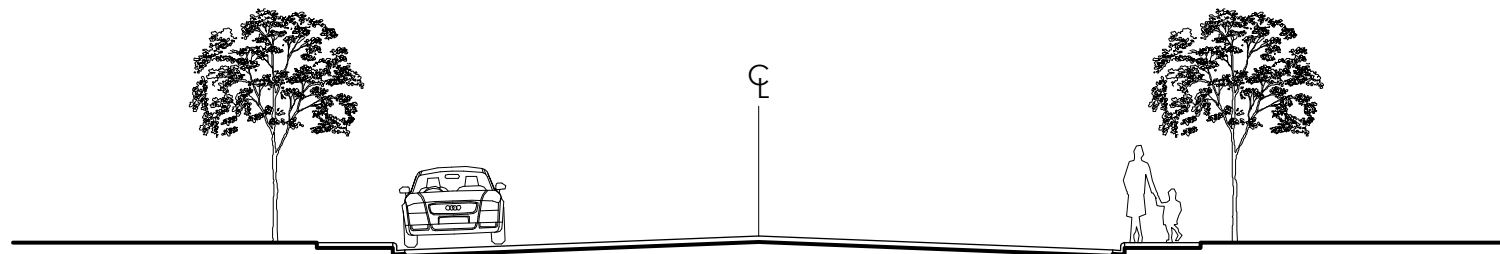


North Waring Road (56' ROW)
Figure 2-2

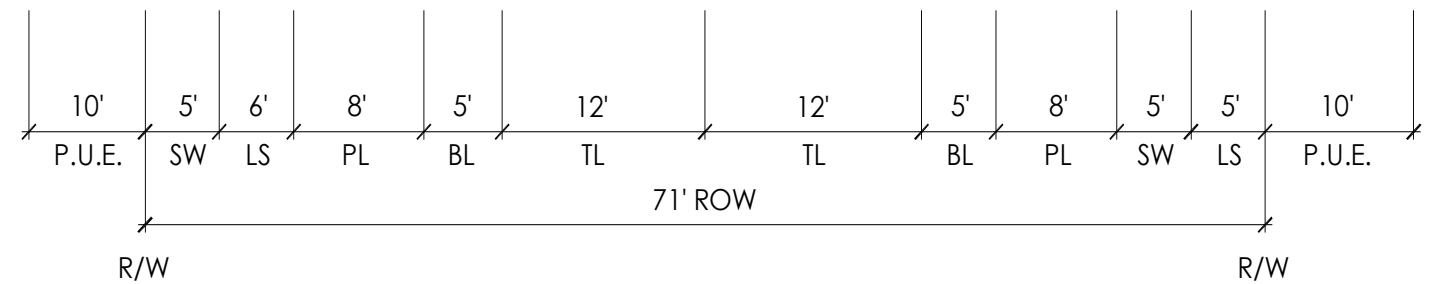
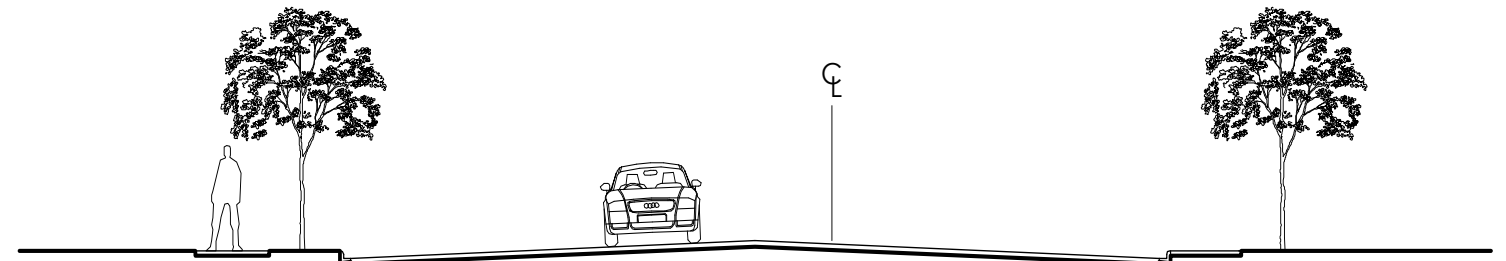
* No Parking Sign to be Installed by Developer



East Tuolumne Road (70' ROW)
Figure 2-4



Quincy Road (60' ROW)
Figure 2-3



East Tuolumne Road (71' ROW)
Figure 2-5

- P.U.E. Public Utility Easement
- T.I.D. Turlock Irrigation District
- S Swale
- SW Sidewalk
- LS Landscaping
- PL Parking Lane
- TL Travel Lane

STREET SECTIONS

- P.U.E. Public Utility Easement
- SW Sidewalk
- LS Landscaping
- PL Parking Lane
- TL Travel Lane

STREET SECTIONS

Chapter 2 - Circulation

2.4 Local Street

Local streets are intended to serve lower volumes of traffic and provide direct access to individual parcels. The design of the local street network and local street sections help define the character of the neighborhood.

Local streets within the Master Plan area should generally include sidewalk, street trees, and a narrower street section that encourage slower traffic speeds, create a pleasant walking environment, and foster social interaction among neighbors. Local street connections should be designed to establish a unified neighborhood throughout the planning area and define a sense of community. Street alignments should be curvilinear to provide an informal, rustic appearance and add to the rural character of the area. Figures 2-8 and Figure 2-9 show the approved street sections for the local streets.

Figure 2-8 shows the street section for the local street that will be used as an entryway into the subdivision. It is the same as the local street section with the addition of an 8-foot wide landscape median which will add to the character of the subdivision.

A number of local streets currently dead-end into the planning area and will need to be extended into the Master Plan area. Wyndfair Drive provides a stub to the southerly boundary of the planning area. Castleview Drive provides a stub to the westerly boundary of the southern portion of the planning area. Valdosta Drive, part of a subdivision south of the four smaller parcels, also provides a stub to the project area. Existing intersections need to be carefully considered to ensure a safe circulation pattern and prevent cut-through traffic.

Three local streets intersect with East Tuolumne Road and North Quincy Road: Swanpark Drive, North Daubenberger Road and Bristol Park. The future circulation pattern will either need to extend these existing roadways into the planning

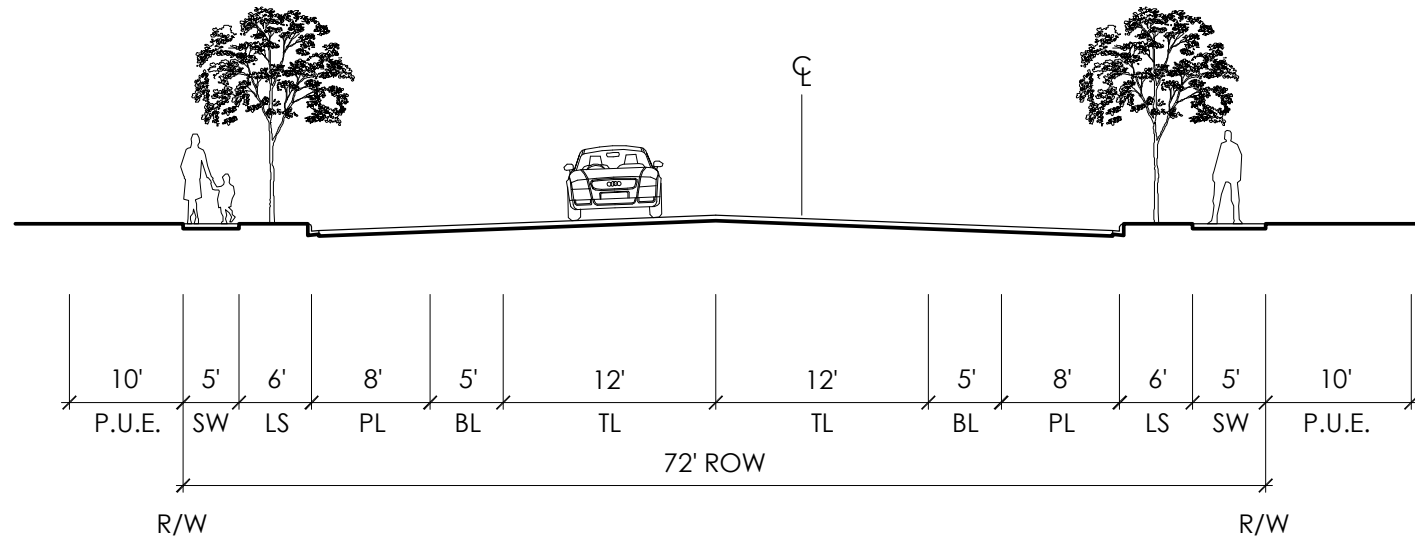
area to form a four-way intersection or will need to offset future intersections a minimum of 250 feet to meet the City's spacing requirements and avoid conflicts between vehicular turning movements.

2.5 Pedestrian/Bicycle Circulation

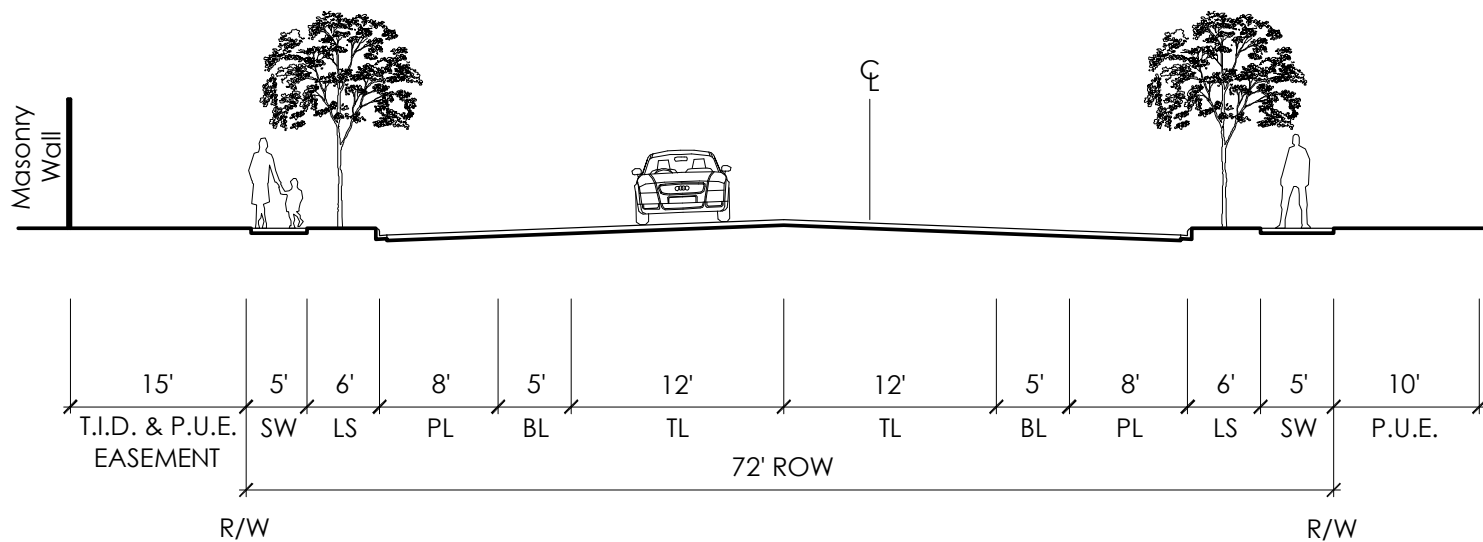
Design amenities such as sidewalks, and bicycle facilities encourage alternatives to vehicular use, promote social interaction, reduce air pollution and add to the overall attractiveness of the neighborhood. The Master Plan requires the inclusion of sidewalks throughout the Master Plan area as well as additional bicycle facilities as illustrated in Figure 2-10 as shown on page 19.

Pedestrian facilities within the Master Plan area shall include curb-adjacent sidewalks on both sides of North Quincy Road and sidewalk adjacent to the landscape strip on both sides of East Tuolumne Road and the western side of North Waring Road. Sidewalks are also required on both sides of all local roads. The pedestrian circulation network should be designed to provide multiple safe connections between the Master Plan parcels that may include street connections and open ended cul-de-sacs. Sidewalks along local roads, East Tuolumne Road and the west side of North Waring Road shall be separated from the street by a landscape strip to create a safer and more pleasant walking environment. Street trees shall be planted within landscape strips in accordance with the landscape design guidelines of this Master Plan to provide shade and relief from hot weather.

Bicycle circulation within the Master Plan area will be accommodated by Class II Bicycle routes on East Tuolumne Road, and North Waring Road and a Class III bike route on North Quincy Road.



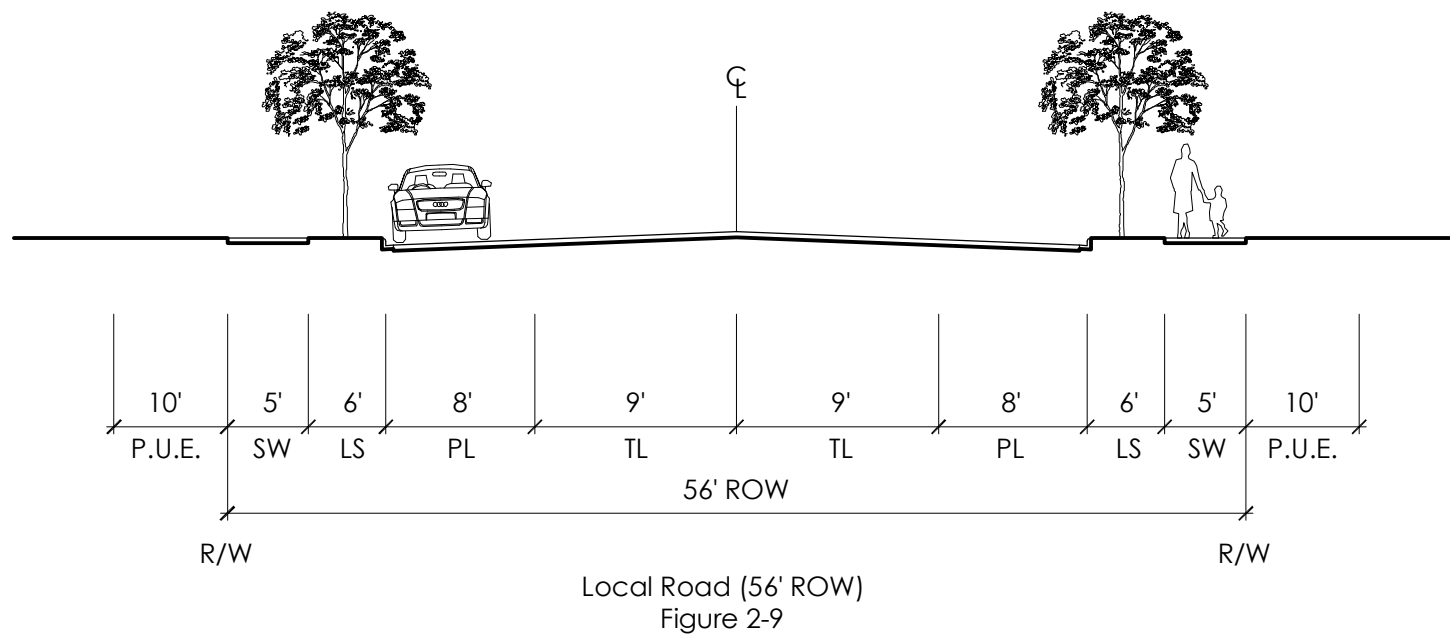
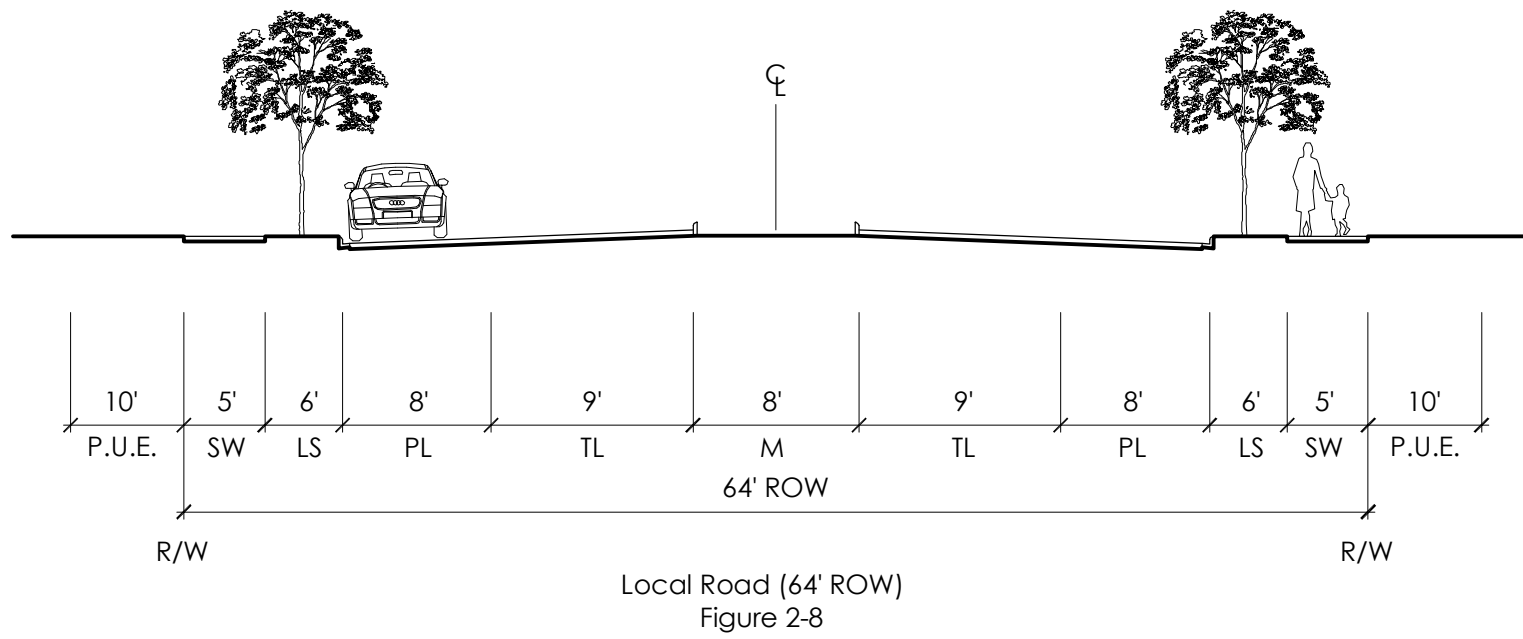
East Tuolumne Road (72' ROW)
Figure 2-6



East Tuolumne Road (72' ROW)
Figure 2-7

- P.U.E. Public Utility Easement
- T.I.D. Turlock Irrigation District
- SW Sidewalk
- LS Landscaping
- PL Parking Lane
- TL Travel Lane

STREET SECTIONS



- P.U.E. Public Utility Easement
- SW Sidewalk
- LS Landscaping
- PL Parking Lane
- TL Travel Lane
- M Median

STREET SECTIONS

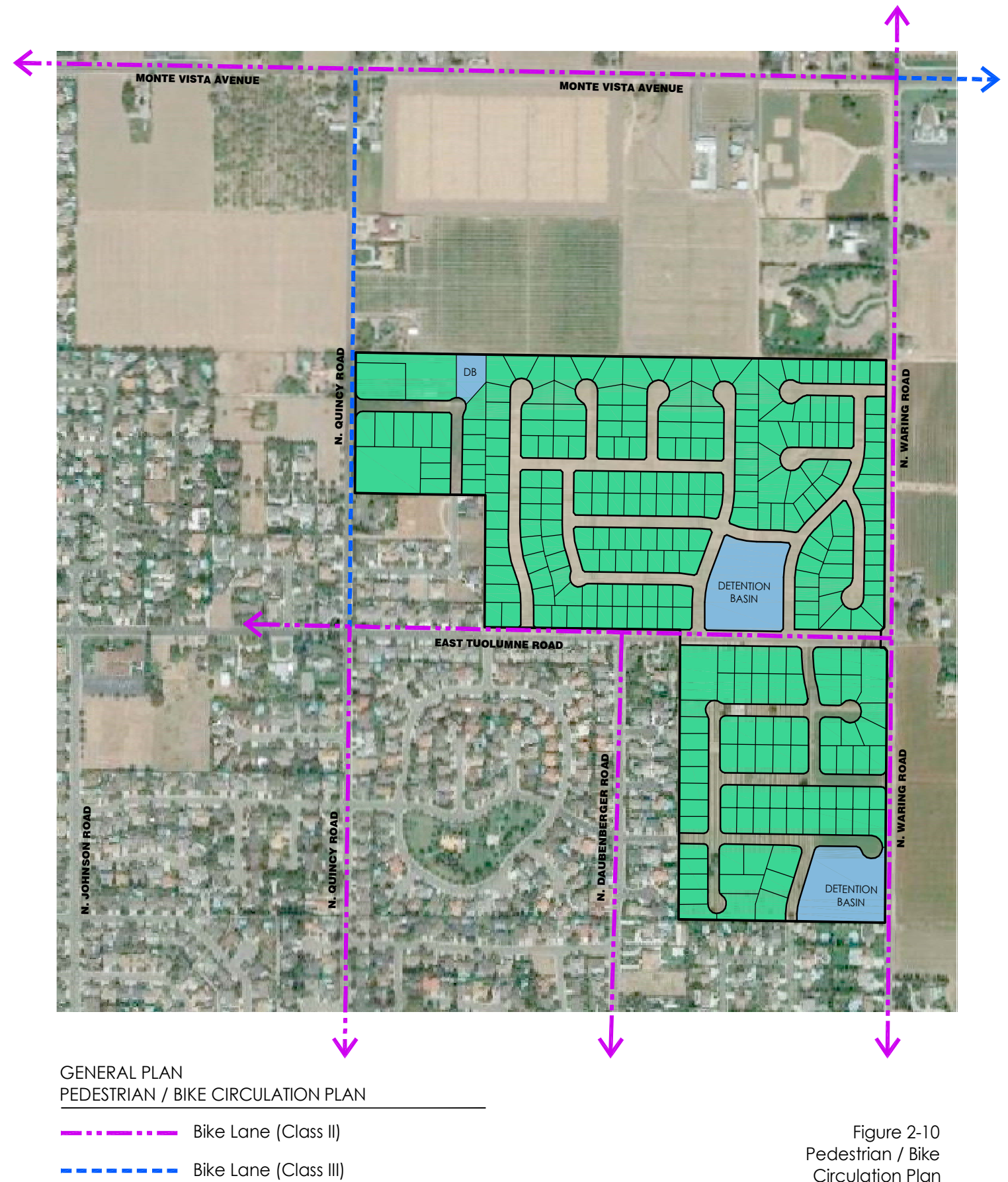


Figure 2-10
Pedestrian / Bike
Circulation Plan

Chapter 3 - Community Character and Design Guidelines

3.1 Introduction

A major theme of the Turlock General Plan is the establishment of a defined urban-agricultural edge to maintain Turlock as an independent community, separated from adjacent communities such as Denair and Keyes by farms and orchards. To achieve this goal, the General Plan City Design element calls for the implementation of an agricultural-urban buffer design.

Another major theme of the Turlock General Plan is the preservation of farmland through more compact development.

The primary purpose of the East Tuolumne Master Plan update is to implement the General Plan's agriculture buffer while increasing the number of lots in the Master Plan to accommodate more compact growth than allowed for in the original Master Plan. This update will increase the units from approximately 200 to 278. This will increase the density while still maintaining larger lots than typically found in residential areas.

The two largest properties on the north side of East Tuolumne Road, APN: 073-013-003 & 073-013-004 will be re-designated to Low Density Residential (RL). To maintain larger lots the density range will be limited from the typical 3 to 7 units per acres to 3 to 4 units per acre.

The other parcels in the Master Plan area will continue to be designated as Very Low Density Residential (VLDR). In addition to establishing large lots, the vision of the Master Plan is to provide design elements that create a graceful and distinct transition from Turlock's urban areas to the large estate lots and agricultural areas in the County. This section provides comprehensive development standards and design guidelines that identify appropriate development standards including setbacks, fencing, landscaping and architecture that create neighborhoods designed to maintain a consistent rural character and ensure successful integration with surrounding uses.

The Master Plan seeks to create neighborhoods with a rural character and strong sense of community. Rural areas are characterized by larger residential lots, large estate homes, a variety of lot widths, traditional architectural styles and larger setbacks. Development patterns should emulate these rural design concepts while encouraging walking, bicycling and social interaction between neighbors.

The following design concepts establish the overall vision for the Master Plan area:

- Increased setbacks, a masonry wall and special landscaping will act as a buffer and serve as a transition between urban and rural uses.
- Architectural styles, entry features, landscaping, and other site amenities will establish a consistent rural character.
- Diversity in lot sizes, lot width, and architectural styles will achieve an attractive and unique streetscape.
- Roadway, sidewalk and bikeway connections will achieve connectivity within the Master Plan area and to existing neighborhoods.



Chapter 3 - Community Character and Design Guidelines

3.2 Site Design

The Master Plan area consists of two land use designations: Low Density Residential (RL) and Very Low Density Residential (VLDR). The intent of this Master Plan is to provide a graceful transition and a buffer between urban uses to the outlying rural uses located in the County by creating a rural atmosphere with an agrarian character. Site design plays an important role in establishing this transition while ensuring successful integration into the existing City fabric. Site design within the Master Plan area should blend rural development patterns with low-density urban development patterns. Future development proposals within the Master Plan area shall meet the following design guidelines:

- a. Lots shall not front onto North Waring Road, a 7-foot tall masonry wall shall be installed and increased rear yard setbacks shall be used to provide a buffer between homes and existing agricultural operations to the east.
- b. Increased setbacks shall be required for APN: 073-013-003 and 073-013-016 along the northerly property line of the Master Plan area to ensure adequate space between the homes and existing agricultural operations to the north in order to minimize conflict with existing agricultural operations.
- c. Setbacks should vary between lots to provide a varied streetscape.
- d. Lot depths and widths should vary throughout the development area to add interest and variety to the streetscape.
- e. Future subdivisions shall meet the development standards set forth in Table 3-1 and Table 3-2.



Table 3-1
Very Low Density Residential Design Standards

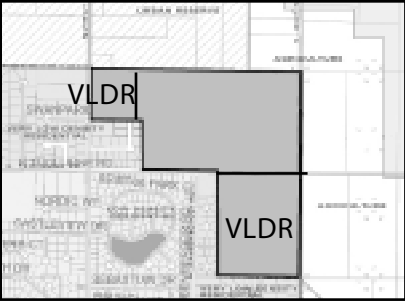

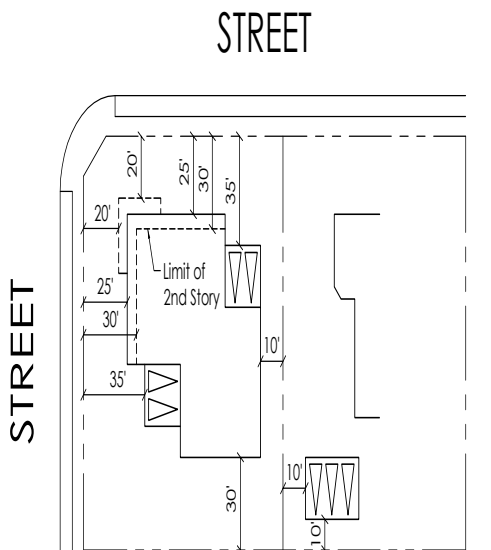
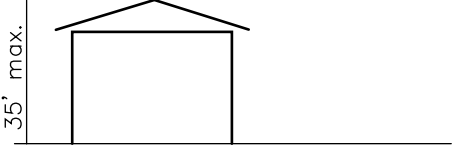
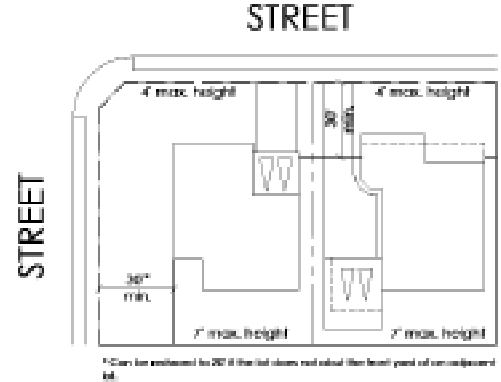
Category	Regulation	Illustration
Land Use Applicable Zoning District Density Range Permitted Uses	VLDR (R-E Zoning) 0.2-3.0 dwelling units/gross acre Refer to City of Turlock Zoning	
Lot Configuration Lot Area Lot Width Depth Curved/Cul-de-sac Frontage Landscape Area Coverage	14,500 square feet minimum; may reduce to 10,000 sf min. along East Tuolumne Rd 90 ft. minimum 100 ft. minimum 60 ft. minimum 30% minimum	
Setbacks Front Yard Porch Living Space (1st story) Living Space (2nd story) Garage Interior Side Yard Porch Primary Building Garage Corner Side Yard Porch Living Space (1st story) Living Space (2nd story) Garage Rear Yard Primary Building Garage	20 ft. minimum ¹ 25 ft. minimum ¹ 30 ft. minimum ¹ 35 ft. minimum ¹ 10 ft. minimum ³ 10 ft. minimum ³ 10 ft. minimum ³ 20 ft. minimum ¹ 25 ft. minimum ¹ 30 ft. minimum ¹ 35 ft. minimum ¹ 30 ft. minimum ^{1&2} 10 ft. minimum ^{1&2}	

Table 3-1
Very Low Density Residential Design Standards

Category	Regulation	Illustration
Building Height Primary Building Accessory Buildings	35' maximum Refer to the Municipal Code Standards	
Porches & Courtyards Depth Width	6 ft. minimum 25% of the length of the front elevation minimum	Refer to Section 3.3.5
Garages Required Spaces Garage Placement	2 spaces/unit minimum Refer to Section 3.3.6	Refer to Section 3.3.6
Fencing and Walls Front Yard (within 30' setback area) Corner Side Yard (within corner side yard setback area) Corner Side Yard ⁴ (Out of setback area) Rear Yard	Refer to Section 3.5 4 ft. maximum height (50% minimum transparency), or 3 feet maximum height if solid 4 ft. maximum height (50% minimum transparency, or 3 feet maximum height if solid) 7 ft. maximum height; shall be setback 30 ft. minimum from property line ⁴ 7 ft. maximum height; shall be open-type fencing or "living wall" when abutting agricultural uses or "urban reserve" zone.	

¹ Increases by 15 feet along North Waring Road.
² Increases by 10 feet when adjacent to agriculture or "Urban Reserve".
³ May be reduced by 5 feet along East Tuolumne Road.
⁴ May be reduced to 20 ft. if it does not abut the front yard of an adjacent lot

Table 3-2
Low Density Residential Design Standards

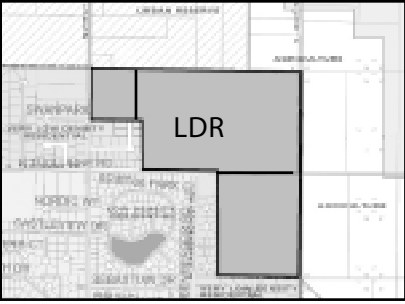
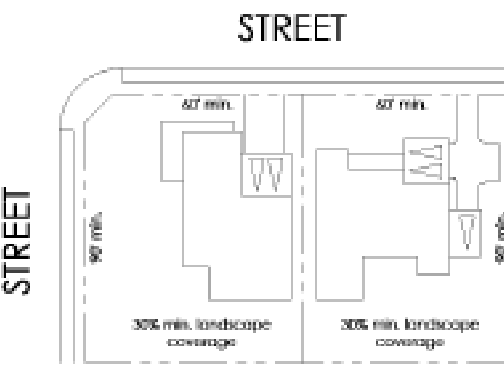
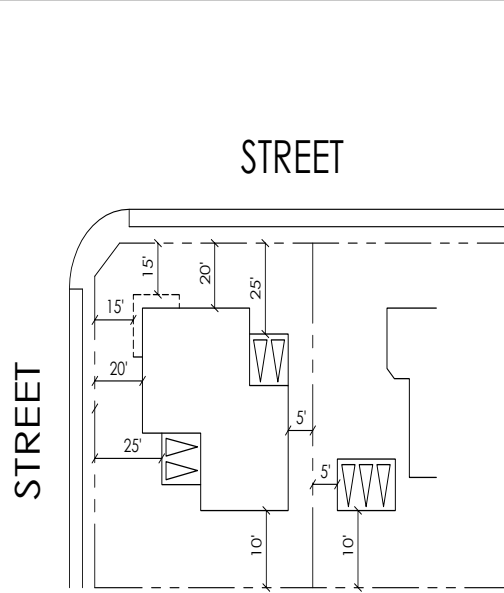
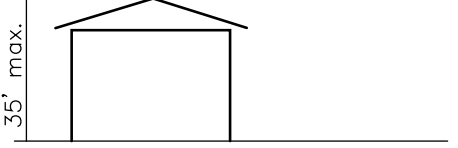
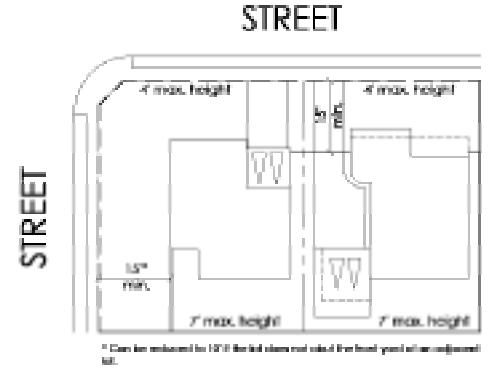
Category	Regulation	Illustration
Land Use Applicable Zoning District Density Range Permitted Uses	Low Density Residential (R-L Zoning) 3-4 dwelling units/gross acre Refer to City of Turlock Zoning	
Lot Configuration Lot Area Width Depth Curved/Cul-de-sac Frontage Landscape Area Coverage	7,000 sf minimum 60 ft. minimum 90 ft. minimum 35 ft. minimum 30% minimum	
Setbacks Front Yard Porch Living Space Garage Interior Side Yard Porch Primary Building Garage Corner Side Yard ³ Porch Living Space Garage Rear Yard Primary Building Garage	15 ft. minimum ¹ 20 ft. minimum ¹ 25 ft. minimum ¹ 5 ft. minimum 5 ft. minimum 5 ft. minimum 15 ft. minimum ¹ 20 ft. minimum ¹ 25 ft. minimum ¹ 10 ft. minimum ^{1&2} 10 ft. minimum ^{1&2}	

Table 3-2
Low Density Residential Design Standards

Category	Regulation	Illustration
Building Height Primary Building Secondary/Accessory Building	35' maximum Refer to Municipal Code	
Porches & Courtyards Depth Width	6 ft. minimum 25% of the length of the front elevation minimum	Refer to Section 3.3.5
Garages Required Spaces Garage Placement	2 spaces/unit minimum Refer to Section 3.3.6	Refer to Section 3.3.6
Fencing and Walls Front Yard (In setback area) Corner Side Yard (In setback area) Corner Side Yard (Out of setback area) Rear Yard	4 ft. maximum height (50% minimum transparency), or 3 feet maximum height if solid 4 ft. maximum height (50% minimum transparency), or 3 feet maximum height if solid 7 ft. maximum height; shall be setback 15 ft. minimum from property line ⁴ 7 ft. maximum height; shall be open-type fencing or "living wall" when abutting agricultural uses or "urban reserve" zone.	

¹ Increases by 15 feet along North Waring Road.
² Increases by 10 feet when adjacent to agriculture or "Urban Reserve", except for lots on APN 073-013-004 (lots adjacent to APN 073-011-012), which is developed with a residence.
³ No reduction in the corner side yard building setback is permitted, even if the lot does not abut the front yard of an adjacent lot.
⁴ The corner side yard fence setback may be reduced to 10 ft. if it does not abut the front yard of an adjacent lot.

Chapter 3 - Community Character and Design Guidelines

3.3 Architectural Design

3.3.1 Massing

- a. All residences shall be designed to have one-main body with well-defined secondary elements such as porches, entryways, window openings, and rooflines.
- b. Front yard setbacks should be varied to create diversity and reduce the apparent massing of homes along the streetscape.
- c. A mixture of single-story and two-story homes is strongly encouraged.
- d. Second story elements should be stepped back or nested into the roof to reduce the apparent mass and bulk of the structure, when appropriate to the architectural style.
- e. Building heights, roof planes and building walls should be varied to minimize the apparent mass of the building, when appropriate to the architectural style.
- f. Architectural elements such as bay windows, balconies, porches, moldings, dormers, pilasters or any other elements that provide visual interest and articulation are encouraged to reduce the apparent mass of the building.
- g. The side elevation for all corner lots shall be enhanced to reduce the apparent mass of the structure by including architectural features that wrap from the front elevation to the side corner elevation. Examples include:
 - Corner related front entry door
 - Wrap-around porches
 - Pop-out side gables
 - Nested or recessed second story
 - Window treatments equal to the front

3.3.2 Articulation

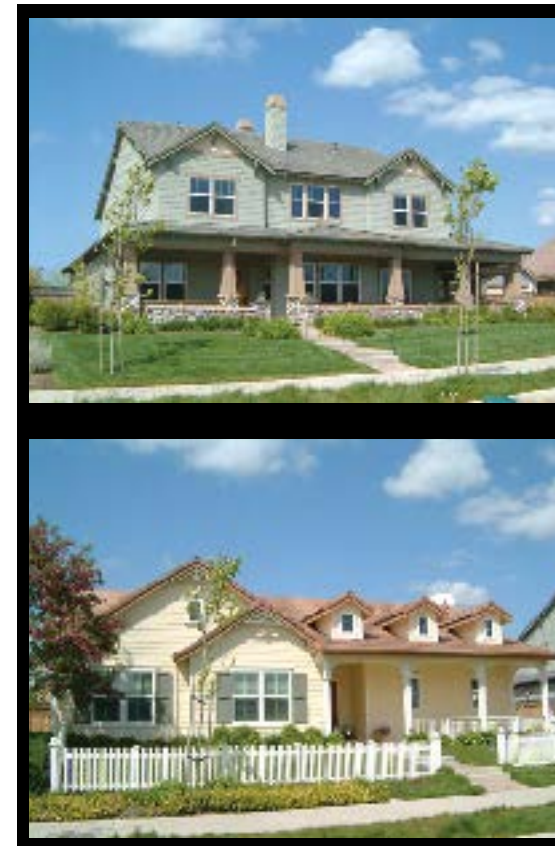
- a. Articulation should include varied massing, wall planes, roof planes, windows, doors, porches, balconies and other design features.
- b. Architectural elements should be included on all four sides of the residence and should include variation in wall planes, roof planes, windows, doors and massing.
- c. Elevations visible from the public right-of-way should incorporate the colors, materials, articulation, and design features used on the front elevation.



Chapter 3 - Community Character and Design Guidelines

3.3.3 Variety and Consistency

- a. Homes throughout the Master Plan area should provide an assortment of building masses, floorplans, rooflines, elevations, and traditional architectural styles consistent with a rural character.
- b. Similar elevations shall not be used immediately adjacent to one another.
- c. Homes immediately adjacent to one another should provide different materials and colors.
- d. When facing or abutting existing residences, new residential architecture should respect existing housing types and architectural styles by providing complimentary architectural style and massing.
- e. Homes throughout the Master Plan area should vary the design, placement, and configuration of garages.



3.3.4 Colors and Materials

- a. Colors and materials, including roofing materials, shall be complimentary to each other and to the surrounding area.
- b. Colors and materials should be earth toned to reduce visual impact and should be consistent with a rural character.
- c. Natural materials such as unpainted stone or brick are encouraged as these materials add to the rural character of the area.
- d. A variety of colors, materials and architectural forms should be carefully applied to all homes to convey the impression of high quality architecture and to create depth and contrast for design features and building articulation.
- e. Colors, materials, and architectural details for individual homes should be carefully coordinated to be harmonious with one another and consistent with the overall architectural style to avoid a piecemeal appearance.
- f. Materials used as major surface treatments at front elevations should be wrapped to side and rear elevations. The appearance of artificial facades shall be avoided. All elevations visible from the public right-of-way shall be enhanced.
- g. Color schemes should be simple and consist of a maximum of three colors per residence.
- h. Bright or reflective colors and materials should be avoided.
- i. Building designs, construction techniques, materials, and appliances that conserve resources are encouraged.
- j. All materials and color changes must occur on an inside corner wall plane.
- k. Exterior wall materials, colors, trim and architectural details shall be consistent on all four sides of the residence.

Chapter 3 - Community Character and Design Guidelines

3.3.5 Porches, Patios, Balconies, and Courtyards

- a. Porches, patios, balconies, and courtyards should be incorporated into the design of all front elevations to provide aesthetic value and encourage social interaction.
- b. Front porches, patios and courtyards should be oriented toward the street and should be welcoming to neighbors and visitors. These elements shall be located in front of the leading edge of the garage.
- c. Architectural elements such as porches, patios and courtyards should be designed proportionally to the overall scale and massing of the main structure.
- d. Courtyard walls should be made of natural looking material and should be consistent with the overall character of the main home. Courtyard walls should not exceed three feet in height.
- e. Front porches should be elevated a minimum of 18 inches above the public sidewalk to establish an appropriate relationship between these spaces.
- f. Porches shall be a minimum of 6 feet deep and extend along at least 25% of the length of the front elevation.



3.3.6 Garage Placement and Driveways

- a. Garage frontages should be minimized through careful placement. Garages should be side loaded, setback from the main living area, placed at the rear of the lot, or otherwise creatively designed to reduce the visual impact of the garage door on the streetscape (See Figure 3-1).
- b. Driveway aprons should be minimized to the greatest extent possible to reduce the amount of impervious surface and provide a more aesthetically pleasing streetscape.



Figure 3-1
Driveways & Garage Configurations

Chapter 3 - Community Character and Design Guidelines

3.3.7 Doors and Windows

- Architectural elements such as doors, windows, dormers, and cupolas should be designed proportionally to the overall scale and massing of the main structure.
- Windows should be placed and designed to maximize views and sunlight exposure.
- Window and door types, shapes, and sizes shall be consistent with the architectural style of the building.
- Main entrances should be oriented toward the street, and should be accentuated with lighting, porches, balconies, courtyards, sitting areas, or other architectural details.



3.3.8 Roofs

- Roofing colors and materials should be consistent with the architectural style of the home and the rural character of the neighborhood.
- Homes throughout the planning area should use a variety of rooflines and pitches such as side gables, cross gables, hipped roofs, and combined hipped-and-gabled roofs.
- Flat roofs and A-framed roofs are discouraged.
- Rooflines shall be varied in height and length to provide articulation.
- Gutters, downspouts, vents and other roof appurtenances should be finished to compliment the materials and colors of the main residence.



3.3.9 Equipment

- Mechanical equipment should be strategically placed and screened from view.
- Roof mounted equipment is not permitted.
- Equipment screening should be designed to blend with the building through use of landscape materials or through use of colors and materials consistent with the main residence.

Chapter 3 - Community Character and Design Guidelines

3.4 Landscaping

Landscaping is one of the most important elements in preserving rural character. Rural landscapes typically consist of informal grouping of trees and plants as well as more organized plantings of agricultural crops. These landscape concepts should be used in the design of gateways, entry features, roadways and front yards. The following landscape guidelines seek to ensure preservation of the area's agrarian character:

- Landscaping shall be used to enhance and reflect the rural character of the Master Plan area and the surrounding rural areas.
- Streets should be heavily landscaped so that mature street trees provide a full canopy over the street and sidewalk. Each residential lot within the Master Plan area shall provide a minimum of 2-3 street trees along its frontage and 2-3 street trees along the side yard of corner lots.
- Trees should be strategically planted to provide shade and cooling efficiency for residences, public spaces and paved areas.
- Paved areas such as patios and walkways should be minimized to allow for natural percolation, reduced runoff and increased landscape aesthetics. When paving is used, impervious paving materials such as raised wooden decks, paving stones, gravel, unmortared brick or stone, or other permeable paving systems are encouraged as an alternative to asphalt and concrete.
- Landscaping materials should consist of native and drought tolerant species that require minimal maintenance.
- Ornamental and specialty plantings that are consistent with a rural character are encouraged as accents to entry and gateway features.

The following list includes City approved street trees as well as other landscaping materials appropriate for use in the Master Plan area.



Street Tree:

(Will be determined by Parks Department on tentative map)

- Chinese Pistache
- Sycamore
- Red Maple October Glory
- Trident Maple

Ornamental Trees:

- Crape Myrtle (Tuscarora or Catawba)
- Red Bud (Eastern or Western)
- Rose of Sharon Hibiscus
- Japanese Maple
- Saucer Magnolia
- Hawthorn Washington Thorn

Small Trees:

- Crape Myrtle
- Sour Gum
- Little Leaf Linden
- Purple Robe Locust
- Fairmount Ginko Biloba (male only)
- Chinese Hackberry
- Red Maple (October Glory or Red Sunset)
- Bradford Ornamental Pear
- Chinese Pistache

Large Trees:

- Bloodhood London Plane
- Scarlet Oak
- Atropunicea European Beech
- Evergreen Ash-Shamel

3.5 Walls and Fences






Walls and fences are used to define property edges, protect privacy, and provide security and safety. Walls and fences may also be used as design elements for front yard landscaping, neighborhood entries and gateways. When used as a design element, the colors, materials, height and type of wall or fence plays an important role in defining the character of the area. Open fencing establishes a rural and welcoming feel while solid fencing conveys a clear message that the area is private and inaccessible. The Master Plan area seeks to create a neighborhood that encourages social interaction among residents and reflects an agrarian character. Therefore, walls and fences visible to the public shall be designed to diminish their visual impact and reflect the rural character of the area.

Design guidelines for walls and fences are as follows:

- a. Walls and fences should be consistent and complimentary throughout the entire Master Plan area.
- b. Walls and fences should be coordinated and complimentary to landscape improvements.
- c. The colors and materials for all walls and fences within the Master Plan area should be natural looking or earth toned to reflect the rural character of the area. Appropriate materials include rustic ledge stone, distressed limestone or other materials approved by the City Planning Director.
- d. Wall and fencing materials and construction should be durable, high quality and low maintenance.
- e. Fences should be designed to discourage vandalism through design, the use of vandal-resistant materials, or careful placement of landscaping.

- f. Fences and walls should not create unsafe areas such as indefensible spaces, hiding places or other areas that cannot be easily viewed by neighbors or patrolled by police.
- g. Residential rear-yard fencing shall not exceed seven (7) feet in height and should be constructed of wood or other materials approved by the City Planning Director. "Good Neighbor" fencing is encouraged.
- h. Front yard residential fencing shall be open such as split rail, wood picket or other decorative fencing. Front yard fencing shall be at least 50% transparent and shall not exceed four (4) feet in height.
- i. Courtyard or decorative walls in front yards may not exceed three (3) feet in height.
- j. Long expanses of publicly visible fences, for example along the northerly property line of the Master Plan area and along North Waring Road and a portion of East Tuolumne Road, shall be planted with vines or other landscaping or be constructed of split face block wall to prevent the placement of graffiti.
- k. The wall along the northern property line shall be a 7 foot tall masonry wall.
- l. The wall along Waring Road shall include pillars or other architectural features to help break up the long expanse of wall. Split rail fencing not to exceed four feet in height, shall be installed in the fifteen-foot landscape area adjacent to Waring Road.

Table 3-3 Wall and Fence Standards and Photos

Fence Type	Permitted Application	Example of Interpretation
Split Rail Fence	<ul style="list-style-type: none"> • 4 feet maximum • Front yard • Side yard • Required in 15' landscape area adjacent to Waring Road 	
Picket Fence	<ul style="list-style-type: none"> • 4 feet maximum • Front yard • Side yard 	
Decorative Wall	<ul style="list-style-type: none"> • 3 feet maximum • Part of an entry feature • Front yard 	
Good Neighbor Fence	<ul style="list-style-type: none"> • 7 feet maximum • Front yard • Side yard 	
Living Wall/ Fence	<ul style="list-style-type: none"> • 7 feet maximum • Front yard • Side yard • Adjacent to North Waring Road and a portion of East Tuolumne Road 	

3.6 Signage

Signage plays an important role in the community by geographically orienting residents and visitors. Signage not only improves circulation but also makes a statement about important locations such as gateways and neighborhood entries. When designed properly, signage can help establish a community's character. When designed improperly, signage can detract from the community's appearance. Therefore, signage should be carefully designed to maintain the rural character of the area and should not dominate the landscape. The following design guidelines are intended to ensure that signage is properly integrated into the overall fabric of the Master Plan area:

- a. Signage should be designed to compliment the rural agrarian character of the Master Plan area and the surrounding rural uses.
- b. Materials and colors of signage should be compatible with the architectural colors and materials of the Master Plan area.
- c. Signage should be carefully coordinated with landscaping, monumentation, walls, fencing and other design elements to ensure a harmonious design.
- d. Signage should not detract from the rustic character of the area.
- e. Street signs should be consistent with City of Turlock standards.
- f. Gateway signs should be consistent with the Turlock Beautification Master Plan.
- g. Signage materials and construction should be durable and low maintenance.
- h. Signs should convey information in a clear and concise manner.



3.7 Lighting

One of the greatest benefits of living in a rural setting is the ability to see stars in the nighttime sky. The design, scale and luminosity of standard urban street lights coupled with extensive landscape lighting can reduce visibility of the nighttime sky and detract from the rural character of the area. Lighting however is necessary to provide a safe and secure neighborhood. Lighting within the Master Plan area should be designed to provide the lowest level required for public safety and should be designed at a pedestrian scale. The following design guidelines are intended to ensure lighting is consistent with the overall character of the Master Plan area:



- a. Streetlights should be spaced to minimize glare and reduce the amount of light to the greatest extent possible without sacrificing public safety. This may be achieved by alternating streetlights from one side of the street to the other.
- b. Intersections, sharp curves, ends of cul-de-sacs, trails, crosswalks and other areas that may pose potential safety hazards for motorists and pedestrians at night should be illuminated.
- c. Use of accent lighting to highlight trails, neighborhood entries, gateways and special landscaping is encouraged.
- d. Architectural and front yard landscape lighting fixtures should be coordinated with the colors, materials and architectural style of the home and should be consistent with the rural character of the Master Plan.

- e. Lighting shall meet the photometric standards established by the City of Turlock.
- f. Streetlights should be designed at a pedestrian scale and should be designed to reflect the rural character of the Master Plan area.
- g. Ornamental landscaping lights and illuminated bollards along trails and in other public areas should be designed to reflect the rural charter of the area and should be consistent with the design of the area's streetlights.
- h. Street lights should be consistent with the City's Standard Ornamental Street Light (See Figure 3-2).



Chapter 4 - Public Facilities and Services

4.1 Public Facilities

The Public Facilities Chapter intends to provide information regarding backbone infrastructure required to service future development within the Master Plan area. In addition, this chapter will identify how the project will be served by the City's fire and police services in accordance with the City's General Plan.

Due to the conceptual nature of the Master Plan document, the following infrastructure plans are intended to be conceptual in nature. Although the general location of infrastructure improvements will be required to be consistent with the Master Plan, additional infrastructure improvements and minor deviations from the conceptual plan may be necessary depending on right-of-way acquisition and the final design of projects within the Master Plan area. The actual size and location for all utilities will be determined by the Tentative Map and will be subject to review and approval of the City of Turlock Development Services and Municipal Services Divisions.

Infrastructure improvements will be subject to current City impact fees and connection fees as determined by the Development Services Department and Municipal Services Departments at the time of Tentative Map approval. Some improvements, including oversized pipes that serve the greater community, may be eligible for reimbursement by the City through the City's Capital Improvement Fund.

4.2 Water

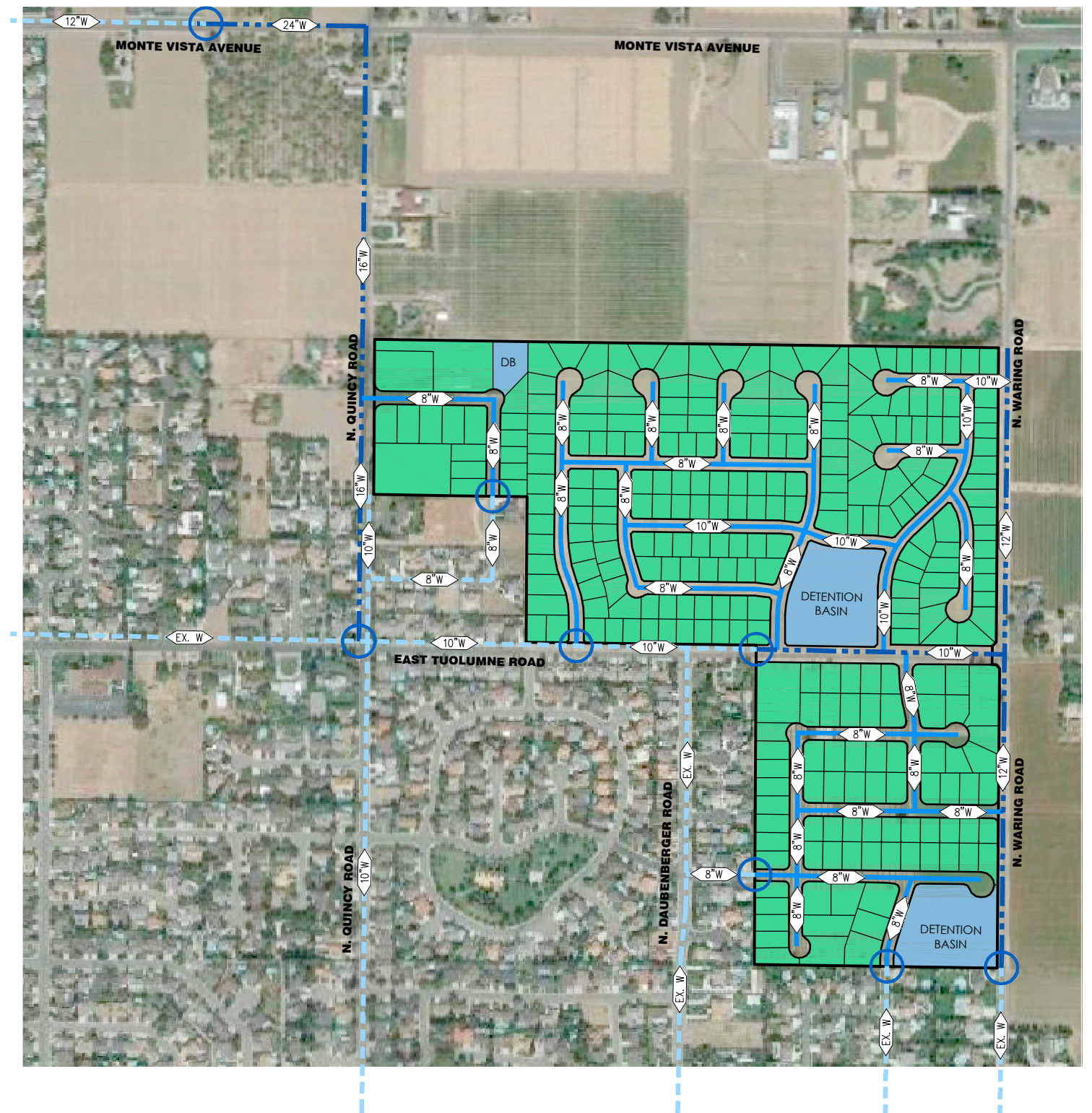
The City of Turlock will provide potable water to future residences within the Master Plan area. The City's current water system consists of domestic wells and a series of looped pipes. The City will also study the use of surface water to provide potable water. The Master Plan area's backbone infrastructure will consist of a series of pipes, ranging from 8-inches to 24-inches, that connect to the City's existing water system and allow for a future connection when a surface water supply is established.

Figure 4-1 on the following page illustrates the conceptual water system for the Master Plan area.

Backbone water infrastructure will be located in North Waring Road, East Tuolumne Road, Quincy Road and Monte Vista Avenue. A main line, consisting of 24-inch pipe, will be installed in Monte Vista Avenue and will connect to a new 16-inch pipe that will be installed in North Quincy Road. The lines in Monte Vista and North Quincy are oversized to provide a future connection to the City's future surface water supply. The difference in cost between this 24-inch pipe and the 12-inch pipe that would normally be needed to serve the project is eligible for reimbursement by the City. An existing 10-inch pipe located in East Tuolumne Road will be extended east to connect to the 12-inch water lines that will be installed in North Waring Road. The pipelines on East Tuolumne Road, and Quincy Road will provide connection points for future pipes within the new roadways of each neighborhood.

The actual alignment of project specific water distribution pipes will be determined by the alignment of future roadways. Figure 4-1 illustrates a conceptual water system consistent with the conceptual circulation plan shown in Figure 2-1, however these alignments may be modified to be consistent with future tentative map proposals. The conceptual system as shown, will consist of 10-inch pipes and 8-inch pipes installed in the streets. Additional connections to the existing water system will be required to loop the water system as shown in North Daubenberger Road.

The City has indicated a possible need for a future domestic well to serve this portion of the City. Although the actual location of this future well has not been determined, the Master Plan area has been identified as one possible location. The Conceptual Water Plan assumes that this well will be located on an alternate site, however, future development may be required to dedicate land for this purpose.



WATER PLAN LEGEND

- Existing Water
- Project Water
- - - Master Plan Water
- Connections to Existing Water

Figure 4-1
Conceptual
Water Plan

Chapter 4 - Public Facilities and Services

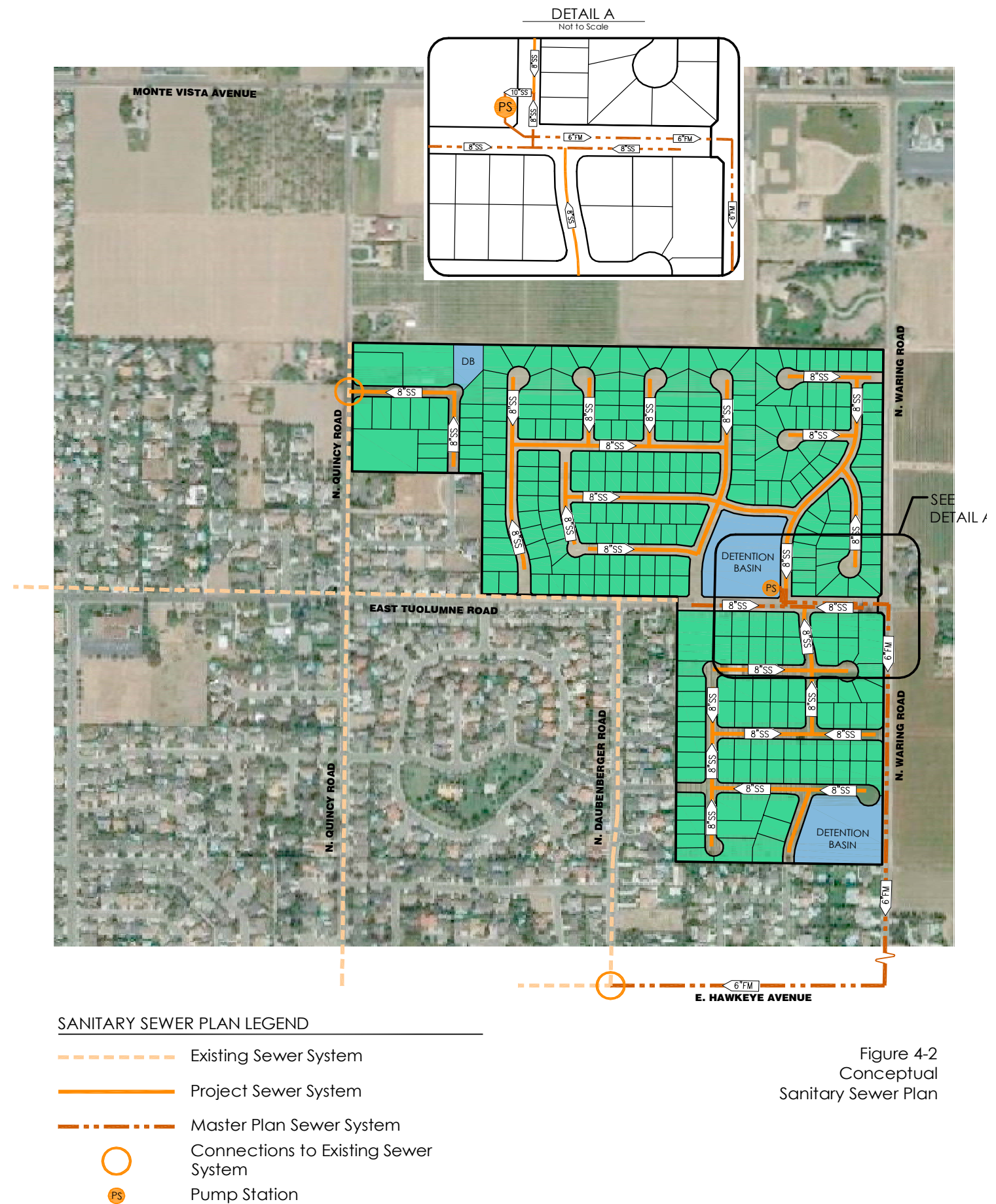
4.3 Sewer

The City of Turlock provides sewer service throughout the City. Sewer service includes collection, transmission and treatment of wastewater through a series of gravity trunk lines and a wastewater treatment plant. This system will be expanded to include the Master Plan area.

The conceptual sewer plan is shown as Figure 4-2 on the following page. The smaller parcels in the northwest corner of the Master Plan area will be served by a system of pipes that tie directly into an existing main in North Quincy Road. The remaining northerly portion of the Master Plan area will be served by a series of pipes that direct wastewater south to an 8-inch main line in East Tuolumne Road and then east into an 6-inch force main in North Waring Road. A pump station will be installed on the north side of East Tuolumne Road, near the detention basin.

The southerly portion of the master plan area will be served by a series of pipes that direct water to a 8-inch line on East Tuolumne Road which leads to the same 6-inch force main in North Waring Road.

A 6-inch force main will be installed in North Waring Road and it will be extended west down East Hawkeye Avenue to North Daubenberger Road, where it will tie into the existing sewer system.



Chapter 4 - Public Facilities and Services

4.4 Storm Water

The City's existing storm water system consists of a series of pipes and detention basins that ultimately release storm water into the San Joaquin River. Catch basins located in streets collect storm water and direct it into pipes within roadways. If the downstream pipes have adequate capacity, this water is then directed through pipes into the overall City storm drain system. In the event of a large storm, however, downstream pipes may not have enough capacity and water is then directed to detention areas and stored within the basin using a control structure. Once downstream pipes regain capacity, the control structure is opened and storm water in the detention basin is released into the overall City storm drain system.

Due to limited capacity of the City's existing system and to maintain water quality within the San Joaquin River, the City of Turlock requires all projects to provide on-site detention facilities capable of storing storm water equal to a 2-inch depth over the entire project site. Based on these requirements, the development within the Master Plan area will need to provide detention facilities capable of storing approximately 15 AF of storm water. The actual size and location of these detention facilities will be determined during the Tentative Map process. The location of detention facilities will depend largely upon the ultimate design of each neighborhood and the availability of land available for detention facilities. Detention facilities may include one large basin that serves the entire area or may consist of smaller basins that serve individual development areas.

Figure 4-3 demonstrates a Conceptual Stormwater System Plan consistent with the Conceptual Circulation Plan. The Conceptual Plan assumes that each area will develop separately. The Conceptual Stormwater System Plan shows 3 separate detention areas, one for each major development area within in the Master Plan area.

Area A

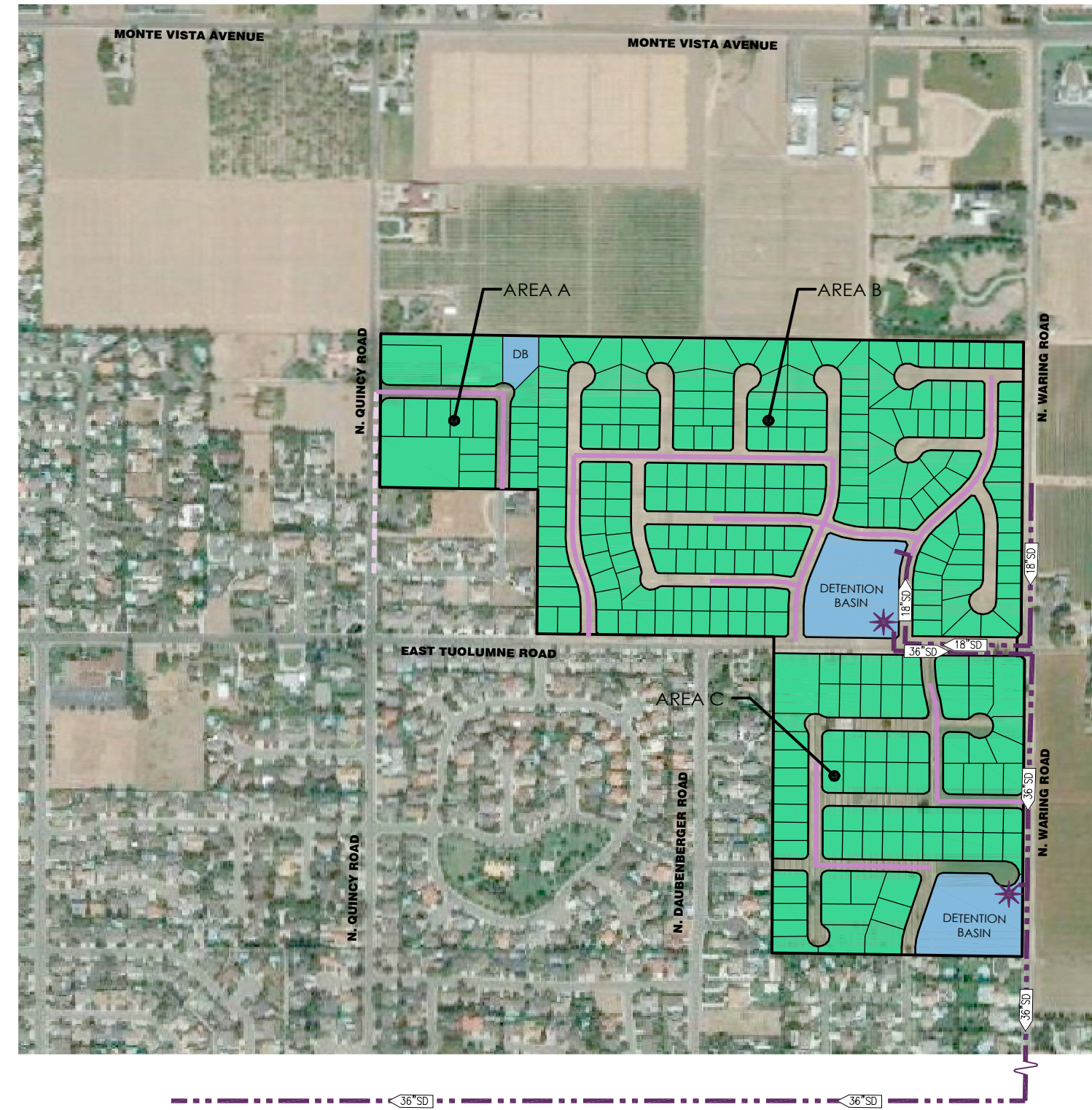
Area A consists of the smaller parcels in the northwest corner of the Master Plan area. Storm drain pipes convey water north to a small detention area. When downstream pipes have sufficient capacity, storm water can then be released into the City's existing storm drain system in North Quincy Road.

Area B

Area B contains most of the Northerly Master Plan development area. Storm drain pipes convey water to a large detention area near the center of the area. When downstream pipes have sufficient capacity, storm water can then be released into a 36 inch storm drain pipe in East Tuolumne Road which will turn south in North Waring Road and feed into a 36-inch storm drain pipe. The 36-inch storm drain pipe will continue south in North Waring Road, turn west in East Hawkeye Avenue and connect to the City's existing storm drain system in East Hawkeye Avenue.

Area C

Area C includes all portions of the Master Plan area south of East Tuolumne Road. Storm drain pipes will convey water to a detention area in the southeast corner of the site. When downstream pipes have sufficient capacity, storm water can then be released into a 36-inch storm drain pipe in North Waring Road. The 36-inch storm drain pipe will continue south in North Waring Road, turn west in East Hawkeye Avenue and connect to the City's existing storm drain system in East Hawkeye Avenue.



STORM DRAIN PLAN LEGEND





-  Existing Storm Drain
-  Project Storm Drain
-  Master Plan Storm Drain
-  Detention Basin (DB) w/ Control Structure

Figure 4-3
Conceptual
Storm Drain Plan

Chapter 4 - Public Facilities and Services

4.5 Police Service

The City of Turlock Police Department provides emergency response, traffic control, animal control, crime prevention and crime investigation as part of its law enforcement program. This area is in the City of Turlock Police Department service area.

The design of future Master Plan neighborhoods should be designed to provide a safe environment with easy access and visibility for police patrolling. To achieve this end, the Master Plan encourages walkways be located along roadways and prohibits these facilities to occur in areas behind homes where they are not easily visible to neighbors and police.

4.6 Fire Service

The City of Turlock Fire Department responds to medical emergencies, fires, hazardous materials spills, public assistance and other emergency calls while managing various operational programs including emergency vehicle management, emergency equipment management, pre-fire planning and facility operations. The Turlock Fire Department also provides fire prevention services and employee training services.

The Turlock Fire Department operates four stations within the City. Each station operates 24 hours a day, 7 days a week.

The City of Turlock Fire Department provides fire protection services to the Master Plan area. Fire Station Number Three, located on East Monte Vista Avenue is the nearest station to the Master Plan area and is located approximately 1.87 miles away. The City of Turlock Fire department considers 3.5 minutes a reasonable response time to respond to fire emergencies in the City. The Master Plan area is located just outside of this response time; therefore the Fire Marshall may require future development within the Master Plan area to provide appropriate mitigation.

Chapter 5 - Financing and Implementation

5.1 Implementation

The East Tuolumne Master Plan will be implemented through the City of Turlock's development review process. This process requires developers to propose development plans in the form of Tentative Maps and Architectural and Site applications that must be reviewed by City staff and approved by the Planning Commission and City Council. The Master Plan serves as a tool City Staff, Planning Commission and City Council can use to determine if proposed development plans are consistent with the vision for the Master Plan area. This vision requires development proposals maintain the Master Plan area as a rural neighborhood that serves as a transition to County lands to the north and east. Although some flexibility is permissible if a project meets the overall intent of the Master Plan, development proposals should substantially conform to the policies and guidelines set forth in previous chapters.

5.2 Approval Process

The following section describes the various steps in the development and approval process

5.2.1 Environmental Review

As required by the California Environmental Quality Act (CEQA) the Master Plan will need to undergo environmental review to evaluate the project's impact on the environment. During the Initial Study process, the City or the City's environmental consultant will evaluate impacts to habitats and species found within the Master Plan area as well as conformance with the General Plan and other City documents, impacts on archeological and historic resources, and impacts to air quality. In order to determine impacts on air quality, a traffic analysis will also need to be completed to determine how many vehicle trips the project will generate per day. Due to the low intensity of development proposed by the Master Plan as well as preliminary analysis of the site, it is anticipated that the Master

Plan will not result in any significant impacts.

If the Initial Study fails to result in significant impacts or if the project's impacts can be mitigated to less than significant, the City will issue a Notice of Intent to adopt a Negative Declaration or Mitigated Negative Declaration. The Negative Declaration or Mitigated Negative Declaration will be subject to a public review period, during which time the public and any other public agencies will have an opportunity to review the document and provide comments. The Negative Declaration or Mitigated Negative Declaration will need to be adopted by City Council as part of the Master Plan's approval.

5.2.2 Master Plan Amendment Adoption

The East Tuolumne Master Plan amendment consists of a General Plan Amendment and a Rezoning action. These actions shall be adopted by ordinance. The Master Plan will establish the land use classifications, development policies, development standards and design guidelines for the Master Plan area. These development policies and standards may differ from the standards and policies contained in the City's existing zoning code and shall supercede those established by the existing Zoning Ordinance. Development policies and standards not covered by the Master Plan shall be determined by the existing Zoning Ordinance.

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5.2.3 Subdivision Approval Process

The subdivision approval process, governed by the Government Code and The City of Turlock Municipal Code, allows land to be subdivided and sold to future residents. Once the Master Plan amendment has been adopted by resolution, property owners and developers seeking to develop within the Master Plan area may submit a Tentative Map and other development applications to the City of Turlock Community Development Division, that comply with the new development standards.

- Tentative Map: Proposed projects within the Master Plan area will be required to show street alignments, the configuration of lots, a lot fit plan, plans for all utilities and the location of proposed open spaces. These plans should be in substantial conformance with the design guidelines set forth by the Master Plan. The City may approve minor deviations from these guidelines and standards provided that the overall intent of the Master Plan is achieved.
- Architectural and Site Application: Proposed projects within the Master Plan area will be required to demonstrate that the project meets the architectural and landscape design guidelines set forth in the Master Plan document. This includes conformance with all required setbacks and development standards. The City may approve minor deviations from these guidelines and standards provided that the overall intent of the Master Plan is achieved.
- Right-to-Farm Disclosures: Developers within the Master Plan area will be required to provide future property owners with right-to-farm disclosures. The purpose of the right-to-farm disclosure is to inform homebuyers that they are purchasing property adjacent to existing agricultural lands and that farming activities may result in additional noise, odor, dust, insects, pesticide use and smoke. Homebuyers must acknowledge the adjacent

farmer's right to continue these farming activities despite the adjacent residential development.

- Lighting and Landscape District (LLD) Disclosure: Developers within the Master Plan area will be required to disclose to future property owners that they are purchasing property within a Master Planned area and that they will be assessed annually through a Lighting and Landscape District (LLD) for financing the repair and maintenance costs associated with streets, street lights, storm drainage facilities, landscaping and other public facilities that directly benefit residents within the Master Plan area.

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5.3 Financing and Maintenance

All improvements within the Master Plan area, including roadway construction and utility installation, will be financed solely by private developers. Some design and construction costs, such as those that have either been oversized to accommodate other areas and future growth or those that have already been planned by the City's General Plan, are eligible for reimbursement by the City (See Public Facilities and Services Chapter).

Long-term maintenance of improvements within the Master Plan area will be financed through a Landscape and Lighting District (LLD). Residents will be assessed a tax that specifically finances operation, maintenance and servicing of roadways, street lights, traffic signals, landscaping, parks and recreation facilities. The LLD will also finance the operation, maintenance and servicing of the detention facilities. The assessment rate and structure of the LLD will be determined by the City Council and shall be based on operation and maintenance budgeting needs for the Master Plan area and the total number of assessable units.